



Lao People's Democratic Republic

Peace Independence Democracy Unity Prosperity

**SOUTH EAST ASIA DISASTER RISK MANAGEMENT
(SEA DRM) PROJECT FOR LAO PDR**

Project ID No: P170945

**Component 1: Integrated Urban Flood Risk Management
In Muang Xay, Oudomxay (ODX) Province (ODX
Subproject)**

**Addendum to the Environmental and Social
Management Plan (A-ESMP) for Additional Works
(AW) for ODX Subproject**

Volume 1 Main Report

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Acronyms and abbreviations

A-ESMP	Addendum to the Environmental and Social Management Plan (A-ESMP) for Additional Works
AF	Additional Financing
AW	Additional Works
BD/CD	Bidding/Contract Document
COC	Code of Conduct on GBA and VAC
CSC	Construction Supervision Consultant
DONRE	Department of Natural Resources and Environment
DOW	Department of Waterways
DPWT	Department of Public Works and Transport
EIA	Environmental Impacts Assessment
ECC	Environmental Compliance Certificate
EDPD	Environmental Research and Disaster Prevention Division (of PTI)
EG	Ethnic Group
EGEF	Ethnic Group Engagement Framework
EGEP	Ethnic Group Engagement Plan
ESHS	Environmental, Social, Health, and Safety
ESMF	Environmental and Social Management Framework
GBV	Gender-Based Violence
GOL	Government of Lao PDR
GRC	Grievance Redress Committee
GRM	Grievance Redress Mechanism
Lao PDR	Lao People's Democratic Republic
LDRM-AF	Lao Disaster Risk Management Additional Financing
M&E	Monitoring and Evaluation
MOF	Ministry of Finance
MONRE	Ministry of Natural Resources and Environment
MPI	Ministry of Planning and Investment
MPWT	Ministry of Public Works and Transport
ODX	Oudomxay Province
O&M	Operation and Maintenance
OP/BP	World Bank Operational Policies
PA	Protected Areas
PDO	Project Development Objective
PIU	Project Implementation Unit
PMU	Project Management Unit
PTI	Public Works and Transport Institute
RAP	Resettlement Action Plan
RPF	Resettlement Policy Framework
SEA	Sexual Exploitation and Abuse
TOR	Terms of Reference

UXO	Unexploded Ordinance
VAC	Violence against children
WB	World Bank

EXECUTIVE SUMMARY

Need for the ESMP Addendum (A-ESMP)

This document is an addendum to the existing Environmental and Social Management Plan (ESMP) for the Oudomxay (ODX) subproject, part of the South East Asia Disaster Risk Management Project for Lao PDR (the Project), prepared to secure World Bank (WB) clearance on proposed mitigation measures for additional works (AW).

Background on the ODX Subproject

The ODX subproject, part of Component 1, aims to reduce flooding impacts in Muang Xay, Oudomxay Province, and enhance the Lao PDR government's capacity for hydro-meteorological services and disaster response. The subproject area covers the Nam Kor River basin and its tributaries in ODX Province and the activities include the rehabilitation of river banks along the Nam Mao and Nam Kor rivers, rehabilitation of Park 2, installation of a small movable weir downstream of Park 2, development of a new Park 1, and installation or rehabilitation of small sluice gates in Muang Xay.

Subproject implementation arrangement and progress. The Department of Waterways (DOW) of the Ministry of Public Works and Transport (MPWT) serves as the Project Management Unit (PMU) for the ODX subproject, while the Department of Public Works and Transport (DPWT) of Oudomxay (ODX) acts as the Project Implementation Unit (PIU). The Public Works and Transport Institute (PTI) provides technical support, training, and capacity building for safeguard measures, undertaking 3-6 month safeguard monitoring, and submitting Environmental and Social Monitoring Reports (ESMR) to the WB. DOW has also mobilized a national Environmental Safeguard Consultant (NESC) to assist in these efforts.

Construction for the ODX subproject is divided into two lots. Lot 1 commenced in June 2021 and was completed in December 2023, while Lot 2, initially scheduled to start in December 2022 and finish in December 2024, has experienced delays due to seasonal rains. Guangdong No.3 Water Conservancy and Hydro-Electric Engineering Board Co., Ltd. (GD3HE) is the contractor for both lots, with ISAN Corporation and the Lao Transport Engineering Consultant (LTEC) (CSC/ISAN), overseeing construction supervision and monitoring compliance with the contractor Environmental and Social Management Plan (C-ESMP).

Safeguard activities are being implemented according to the C-ESMP approved by PMU, which was prepared, reviewed, and monitored by the CSC/ISAN while the WB providing comments on the progress reports as necessary. The C-ESMP for Lot 2 was approved by the PMU in March 2023 and the contract is expected to be completed in 2025. In March 2024, DOW proposed additional works (AW) for the ODX subproject, with further details provided in Chapter 2 and an estimated cost of about US\$1 million.

Scope of the Proposed AW and the A-ESMP

In 2022, DPWT-ODX requested project support to enhance the stability of the existing embankment and address flooding in Mouang Xay, covering five items with an estimated cost of about US\$1 million. Given the similarity in locations and nature to the proposed AW and results of the safeguard screening, it was agreed that the mitigation measures identified and implemented

as part of the ODX subproject's ESMP will be applied to the proposed additional works (AW), incorporating recent safety improvements suggested by the WB. The current contractor will carry out the AW, supervised by the existing CSC/ISAN, and the Contractor ESMP (C-ESMP) will be updated to include these proposed AW and new safeguard activities after WB clearance of this ESMP (A-ESMP).

Location and Design of the Proposed AW

The proposed additional works (AW) for the ODX subproject are located in Mouang Xay, Oudomxay Province, along the Nam Kor and Nam Mao rivers (Lot 2 area) with an estimated construction cost of about US\$1 million and with a contract extension of approximately six months. The AW activity includes the following five items:

- Bank Protection along left side of the Nam Mao River: L=2.70 km, 5 Pipe Culverts
- Pavement for the Walk Path on the Levee Crest: L=2.00 km, W=3.0 m
- Scouring Protection at 2 Bridges: Railway Bridge and Nam Kor Bridge No. 4
- Access Road cross the Nam Mao River: L=155 m, W=4.0 m
- Streetlight at the Riverside Park 1: 15 Points

Summary of Potential E&S Impacts and Mitigation Measures

In accordance with the ESMF requirement for the LDRM-AF Project, a safeguard screening for the proposed AW of the ODX subproject was conducted, with results detailed in Attachment 2. The impacts and mitigation measures for the AW are expected to be similar to those for the existing ODX subproject and are briefly summarized below while more details are provided in Chapter 3 and Attachment 2.

a) Positive impacts

The proposed AW for the ODX subproject is expected to yield positive environmental and social impacts in a longer term, similar to those identified in the ESMP of the ODX subproject. Oudomxay Province has a history of devastating flash floods, with severe infrastructure damage and loss of life. The proposed structural measures, such as riverbank protection, drainage capacity expansion, and flood risk management, will benefit Muang Xay residents by safeguarding properties and livelihoods, enhancing the environment, and promoting tourism. These measures will also provide socioeconomic, health, and ecological benefits by preventing siltation, sedimentation, and flood-related losses. The subproject aims to create a safer and healthier environment for those previously affected by flash floods. Direct beneficiaries include the 98,000 residents of Muang Xay, while national and local stakeholders will gain from capacity-building activities. Indirect beneficiaries encompass travelers, the broader population receiving improved weather and early warning information, and those benefiting from enhanced institutional capacities for disaster risk management.

Given that location and nature of the activities of the proposed AW will be similar to those of Lot 2, implementation of the proposed AW will enhance effectiveness and sustainability of the subproject structures and/or improve safety of Lot 2 facilities.

b) Negative Impacts and Mitigation Measures

The overall risk and impact of the ODX subproject, including the proposed AW is considered moderate, and the negative impacts will be mitigated through the implementation of the

subproject ESMP including the mitigation measures proposed in 2024 to strengthen measures related to safety of the subproject sites, traffic management, and community health and safety. The proposed AW will adhere to the same national legislations and the subproject ESMP as agreed with WB. Chapters 3 and 4 provided a summary of the ESMP of the ODX subproject.

According to the safeguard screening results, impacts during the preconstruction and operational phases of the proposed AW are expected to be minor. The proposed AW will not require land acquisition and resettlement of private land and structures and will also not involve any vulnerable ethnic group so preparation of any Resettlement Action Plan (RAP) and an Ethnic Group Development Plan (EGDP) will not be required.

Consultation and Information Disclosure and GRM

During the preparation of the ESMP for the ODX subproject, consultations were conducted from May 11-29, 2020, involving 747 participants (287 female) from affected households, village authorities, and various organizations. The results were included in the ESMP report, which was disclosed on the MPWT website. As part of the ongoing consultations and involvement of local authorities and communities during the construction and training phases, additional consultation with village authorities on the proposed AW was conducted in early 2024. Regular consultation and training will be conducted throughout the subproject implementation until the subproject closing date.

The Project's Grievance Redress Mechanism (GRM) aiming to ensure accessible procedures for addressing project-related concerns, detailing complaint submission methods and adhering to Lao PDR's laws and decrees governing complaint resolution, with robust monitoring under the ODX subproject will be continued. For the AW, these principles and procedures, covering worker conditions and social aspects, will be implemented and monitored, utilizing monitoring forms aligned with the ODX subproject's ESMP.

ESMP Monitoring, Reporting and Training

The implementation of the proposed AW will follow a structure similar to that outlined in the ODX subproject's ESMP. The CSC will submit monthly progress reports and quarterly safeguard monitoring reports to DOW (with copies to PTI). DOW/PTI, in turn, will compile and submit the Project Environment and Social Monitoring Report (ESMR) to the WB every six months.

Additional safeguard trainings for the proposed AW will be prepared and implemented by DOW/PTI. This is to ensure that adequate training and budget are allocated for the safeguard implementation including those related to the proposed AW.

On budget, it is estimated that an additional cost of about \$28,000 will be required to cover the cost for training, monitoring, consultants, and consultation with LA/LC and ensure their active participation

1 INTRODUCTION

1.1 Need for the ESMP Addendum (A-ESMP)

This documents. This document is an addendum to the existing Environmental and Social Management Plan (ESMP) of the Oudomxay (ODX) subproject which is being implemented as part of the South East Asia Disaster Risk Management Project for Lao PDR and its additional financing (LDRM-AF or the Project) being implemented with financing from the World Bank (WB)¹. It is prepared to secure WB clearance on the proposed mitigation measures to be implemented for the additional works (AW) being proposed by the Department of Waterways (DOW) of the Ministry of Public Works and Transport (MPWT) responsible as the Project Management Unit (PMU) of the LDRM-AF project including the ODX subproject. Background for the ODX subproject and the safeguard documents is briefly described below.

1.2 Background on the ODX Subproject

The ODX subproject is part of the Component 1 which has been designed to reduce the impacts of flooding in Muang Xay of Oudomxay (ODX) Province and enhance capacity of the Government of Lao PDR (GOL) to provide hydro-meteorological services and disaster response. The ODX subproject area covers the Nam Kor River basin and its tributaries, especially Nam Mao) in ODX Province (Figure 1-1). Scope of the ODX subproject include (i) rehabilitation and improvement of the river bank along the Nam Mao and Nam Kor rivers in priority area for Xay district, (ii) rehabilitation of Park 2, (iii) installation of a small movable weir downstream of Park 2, (iv) development of a new Park 1, and (v) installation and/or rehabilitation of small sluice gates in Mouang Xay (see Table 1-1 and Figures 1-2 and 1-3 below).

Table 1-1 Scope of Works to address flooding in Muang Xay

Scope of Works	COI	Budget (USD)
<p>Lot 2 consists of 4 works as follows:</p> <ul style="list-style-type: none"> i) River works: Dike, riverbank-protection, and excavation on the river bed and channel widening: <ul style="list-style-type: none"> - Nam Mao River (upstream): Right side =3.38km (from the Nam Mao Bridge to 3 Rivers Junction) - Nam Kor River (upstream): Left side=3.46km (from the Nam Kor bridge 5 upstream to Nam 	Ranging from 1m to 150 m identified from the river bank alignments. The Lot 2 works occur in 10 villages including B.	8,818,465.86

¹ The Project (LDRM-AF) aims to reduce the risk of flooding and enhance disaster risk financing capacity of Lao PDR and consists of 5 components: (1) Integrated Urban Flood Risk Management being implemented by the Department of Waterways (DOW) of the Ministry of Public Works and Transport (MPWT); (2) Hydromet Modernization and Early Warning Systems being implemented by the Department of Meteorology and Hydrology (DMH) of the Ministry of Natural Resources and Environment (MONRE); (3) Financing Planning for Disaster Resilience being implemented by the State Reserve Department in the Ministry of Finance (MOF); (4) Knowledge and Coordination being implemented by the Department of Planning of Ministry of Planning and Investment (MPI); and (5) Contingent Emergency Response Component. In late 2019, additional fund of about \$25 million has been provided to increase scope of the Component 1 implementation to cover priority investment in Luang Phabang (LPB) and Bolikhamxay (BKC) while additional fund has also been provided for investment in Oudomxay (ODX) Province.

Scope of Works	COI	Budget (USD)
Kor bridge 4 downstream) - Nam Kor River: L=1.64 km, Right side (from the 3 Rivers Junction ~ Nam Kor bridge 2) ii) Riverside Park 1 (1.3ha): Construction of new River Park 1 iii) Drainage and Flap Gate (16 gates): Construction of 16 small flap gates to improve effectiveness of urban drainage.	Nasao, B. Namy, B. Nalao, B. Navannoy B. Thin, B. Vanghai, B. Pasak, B. Montai, B. Longkordeau and B. Jeng.	
Total		8,818,465.86

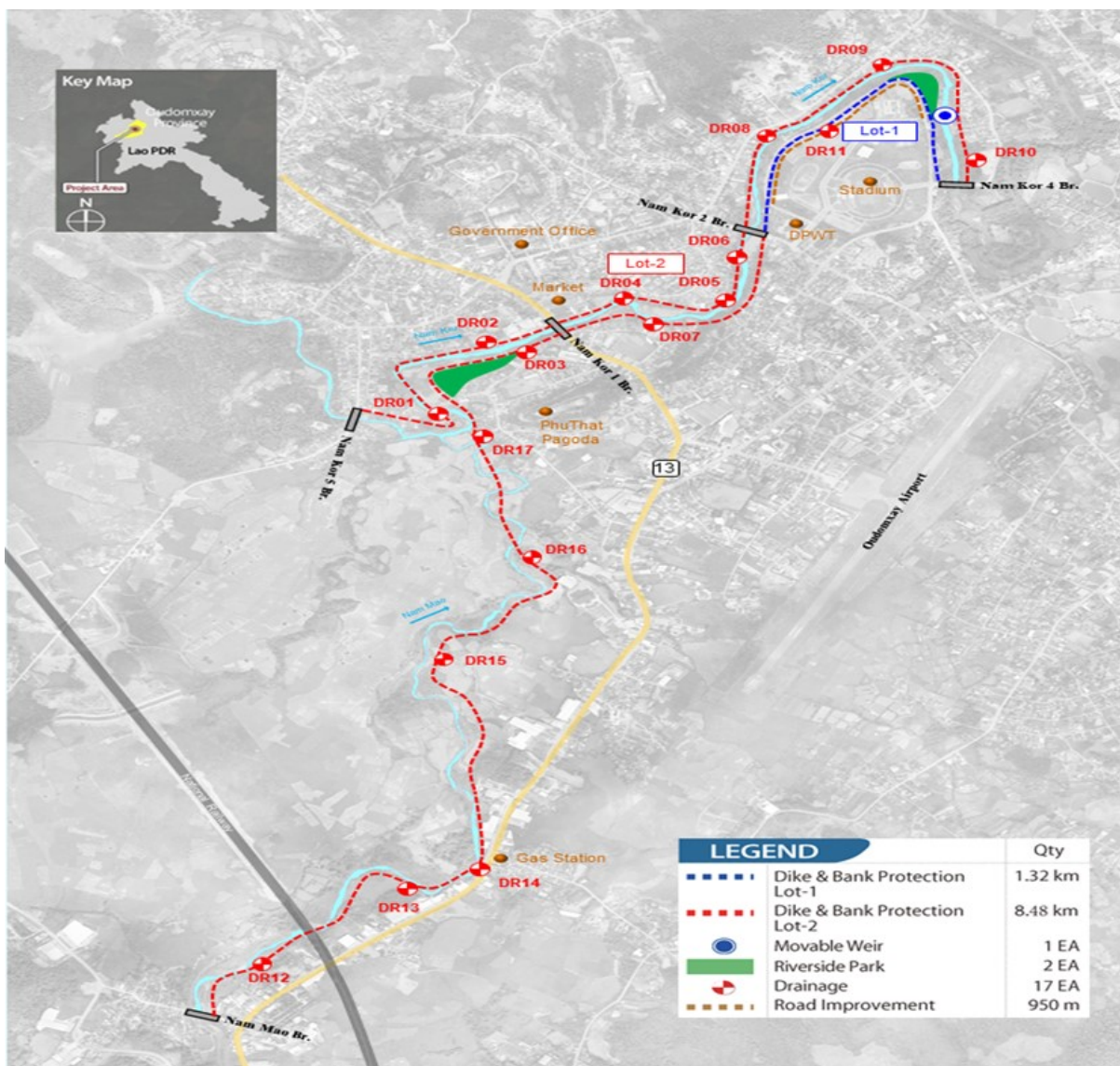


Figure 1-1 General Plan for the total works (Lot 1 and Lot 2) (US\$16.2M)



Figure 1-2 Overall Project Location

Implementation arrangement. The Department of Waterways (DOW) of the Ministry of Public Works and Transport (MPWT) is the Project Management Unit (PMU) while the Department of Public Works and Transport (DPWT) of ODX (as the Project implementation Unit or PIU) is responsible for implementation in ODX. The Public Works and Transport Institute (PTI) provide technical support to PMU, PIU, and local authorities and local communities (LA/LC) during the implementation, training, and capacity building of safeguard including undertaking 3-6 months safeguard monitoring and submit a safeguard monitoring report (ESMR) to WB. DOW mobilizes a national Environmental Safeguard consultant (NESC) to assist DOW and PTI.

Construction of the proposed works for ODX subproject are being made in 2 lots (Lot 1 and Lot 2 – see Figure 1-2) as described in Table 1-1 and Figure 1-2 below. The construction of the Lot-1 was started in June 2021 and 100% completed in December 2023. The construction schedule of Lot 2 was to start in December 2022 and be completed in December 2024 (duration of 24 months), however there are some delays due to early-late raining season in 2022-2023. The contraction contractor for both Lot-1 and Lot-2 is Guangdong No.3 Water Conservancy and Hydro- Electric Engineering Board Co., Ltd., (GD3HE). ISAN Corporation (ISAN) in association with the Lao Transport Engineering Consultant (LTEC) was recruited by PMU (as the construction supervision consultant or CSC) to assist PMU and PIU for construction supervision as well as day-to-day monitoring and monthly reporting including those on C-ESMP compliance and performance of contractor for both Lot 1 and Lot 2 activities from June 2021 to December 2024.

The safeguard activities are being implemented according to the contractor Environmental and Social Management Plan (C-ESMP) submitted to and approved by DOW. The C-ESMP for both Lot-1 and Lot-2 were prepared, reviewed, and monitored by the CSC/ISAN while the WB task team provides comments on the CSC/ISAN implementation progress reports and/or the C-

ESMP as requested by PMU. The C-ESMP for Lot-2 was approved by PMU with the conditions on 14 March 2023.

In March 2024, DOW proposed for the additional works (AW) for ODX subproject. Scope and locations of the proposed AW is described below while more details are provided in Chapter 2 and a cost estimate will be about US\$1 million.

1.3 Scope and Location of the proposed AW and the A-ESMP

In 2022, DPWT-ODX (as the PIU) requested for Project support to address stability of the existing embankment and flooding in Mouang Xay covering 5 items costing around 1.2 million (see details on the activities and location in Chapter 2). Given the locations and nature of the proposed AW are similar to that being implemented under the ESMP of the ODX subproject and after undertaking the safeguard screening, it has been agreed that the mitigation measures identified in the ESMP of the ODX subproject will be applied to the proposed AW including the safety measures proposed to improve safeguard measures recently comments by WB. Given that the proposed AW will be constructed by the current contractor (and supervised by the current CSC/ISAN), the Contractor ESMP (C-ESMP) will also be updated (as an addendum to the current C-ESMP).

Scope of the A-ESMP. Chapter 2 provide brief description of the proposed AW (more details are in Attachment 1) while Chapters 3, 4, and 5 briefly identify key actions that have been and/or to be carried out during implementation of the proposed AW regarding the safeguard screening and key issues/proposed mitigation measures, including those related to consultation, grievance redress mechanism (GRM), implementation monitoring, training, and budget. More details are provided in attachments.

2 DESCRIPTION OF THE PROPOSED ADDITIONAL WORKS (AW)

2.1 Location and Design of the Proposed Additional Works

The proposed AW for ODX subproject is located in Mouang Xay of Oudomxay (ODX) Province on the Nam Kor and Nam Mao Rivers and will cover five villages including Ban Namy, Ban Nalao, Ban Nawannoy, Ban Thin and Ban Longkordeua (see Figures 2-1 below and Attachment 3, Figure A3-7). Table 2-1 present the activities and estimated cost for the proposed AW. It is estimated that the construction cost will be about US\$1 million and the contract extension will be about 6 months. Scope of the activities can be highlighted below. Given limited funds, the proposed channel improvement downstream of Nam Kor bridge #4 will not be conducted as part of the proposed AW. Details on design and estimated costs of AW are presented in Attachment 1.

Table 2-1 Additional Work Items

No.	Description	Remarks
1	Bank Protection along left side of the Nam Mao River:	L=2.70 km, 5 Pipe Culverts
2	Pavement for the Walk Path on the Levee Crest:	L=2.00 km, W=3.0 m
3	Scouring Protection at 2 Bridges:	Railway Bridge and Nam Kor Bridge No. 4
4	Access Road crosses the Nam Mao River with a Levee Passage. See Attachment 1, Figure A1-11	L=155 m, W=4.0 m
5	Streetlight at the Riverside Park 1:	15 Points

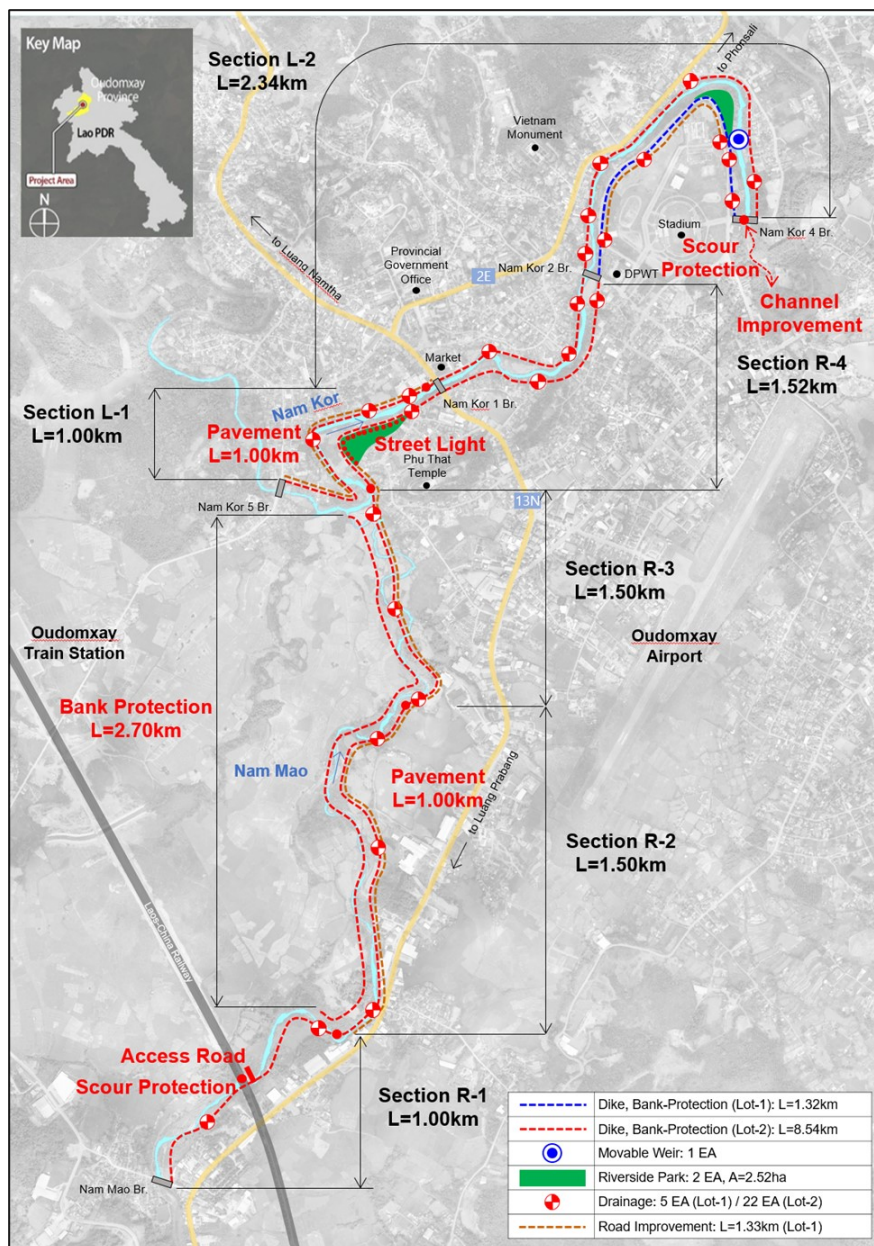


Figure 2-2 Location Map for the Additional Work Items

2.2 Construction Materials, Borrow Pits, and Quarry Site

For the proposed AW, the construction materials will come from the existing borrow pits and quarries site while the disposal sites will also be the same as those being used for Lot2 (see locations and transportation routes in Attachment 3).

The major construction materials on embankment for riverbank protection are 9,496m³ of soil which will be from the excavated soil; 27,995 m³ of bedding rubble and bedding rip-rap and 454.5m³ head concrete from existing quarry sites and concrete plant. The laboratories tests have been performed to check the suitability of the materials from selected borrow pits, excavated soils, and quarry sites.

Borrow pits. The existing borrow areas in Nasao and Nalao villages with about 3.2 km to the nearest protected area and 16km to the project site (Attachment 3, Figure A3-2). Both borrow pits are private land and an agreement with landowners with condition that the site shall be stabilized and leveled or improved the pit to be residential land before returning back to the village after the use. The main transportation route will be using the Nation Road 13 North (NR13N) and Road 2E.

Quarry sites/concrete plants: The rip-rap to be mainly used for the foundation of bank protection, and toe protection will be provided from quarry sites and will be tested to see if the major material properties such as unit weight, uniaxial compressive strength, and etc. are suitable for the construction materials. The quarry site and batch plant will be sourced from existing concession sites used for Lot 2 which located in Ban Houysou with 11km distance from the Project site (Attachment 3, Figure A3-3). The site is not located in the protected area with about 11km distance to the nearest protected area and about 12km to residential area and project site. The main transportation route will be using the Nation Road 13 North (NR13N) and Road 2E.

Transportation routes. There are natural trees, bushes, residential and sensitive receptors such as community areas along transportation routes from the quarry sites and borrow pits to the AW site. The impacts and mitigation measures are provided in Chapter 3. Agreement with land owner was obtained and included in the C-ESMP to be amended by the contractor and included in the safeguard monitoring report of ODX subproject and/or any report as required by WB. The transportation will be not carried out during peak hours (commuting times to work or school). All dump trucks carrying out surplus soils will have to be fully covered while the drivers will be required to respect driving speed and avoid using horns in area sensitive to noise and vibration such as hospitals, schools, and temples. DPWT of ODX and the safeguard monitoring working group (SMWG) chaired by DPWT will facilitate discussion and conduct consultation with land owner and facilitate for agreement between contractor and land owner. The contractor will be responsible for ensuring that the final conditions of the borrow-pit are accepted by the land owner and this condition will be part of the C-ESMP to be implemented by the contractor and monitored by the CSC/ISAN-LTEC.

Excavated Soil Utilization Plan: Based on the recent information from the detailed design team, about 32,873m³ of excavation soil will be generated of which 9,496m³ will be reused for embankment and 23,377m³ will be disposed at the existing land filling sites and disposal

areas used for Lot 2 (Attachment 3, Figure A3-4 and Figure A3-5). There are local people requested for the surplus soil to fill or level up their land. PIU has established key criteria for selection of soil disposal site and conducted consultation and obtained agreement with land owners that provided in the Lot2 ESMP. key criteria included as follows: (a) The site will be located within the 2km distance to site or not more than 3km if no any suitable location within 2km distance; (b) The project affected households will be priority to receive the surplus soil for leveling up or filling their empty lands for future use as residential land; and (c) the following areas should be avoid: mountainous, high slope areas prone to erosion, environmentally sensitive areas such as water sources, wetland, sensitive forest. Also, removal of tree will be avoided and minimized.

To make an agreement between PIU and land owner, the following terms are being used during consultation and negotiation process:

- Reserve 5 m from each side of drainage channel for potential development and improvement;
- The land owner will provide access to the disposal area including clearing any barriers such as some trees, firewood storage, animal hut, etc. without compensation. Any issue occurred during the implementation of the disposal activities; the PIU will conduct regular consultations with the land owner to ensure compliance with the Project implementation procedures.
- An agreement will be provided to the contractor to comply with the agreed terms with the land owners.

In general, the land owner and the PIU agreements follows the criteria established above. The land owner will provide access to disposal area including clearing any barriers such as some trees, firewood storage, animal hut, etc. without compensation while the PIU will conduct regular consultations with the land owner to ensure compliance with the agreement and/or solve any potential conflicts during actual disposal of the spoil. Additional analysis for organic contents of the excavation soil will also be conducted to ensure that quality of the spoil will not cause adverse impacts to local communities.

Transportation of surplus soils. The sites for disposal of 23,377m³ surplus soils are located with a distance ranging from 0.10km to 0.2km; therefore, temporary onsite storage of the excavated soils is not required. The excavation will be carried during the dry season by using backhoe and will be directly placed in the dump trucks with a capacity vary from 15m³ to 20m³. Due to the short distance from the subproject site to the disposal sites, it is estimated that one dump truck will make 5-10 trips per day and the transportation will not carried during peak hours (commuting times to work or school). All dump trucks carrying out surplus soils will have to be fully covered while the drivers will be required to respect driving speed and avoid using horns in area sensitive to noise and vibration such as hospitals, schools, and temples. Attachment 3 (Figure A3-6) presents the transportation routes for the proposed AW.

2.3 Construction Plan and Schedule

Taking account of the remaining construction period and quantities of the proposed AW, it has been recommended that 6 months will be added to the construction period of Lot 2. Attachment 1 also provides information on estimated costs for the proposed AW. A Contractor-Environmental and Social Management Plan (C-ESMP) for Lot 2 was prepared by the Contractor and reviewed and approved by DOW/PMU in March 2023 with support from the Construction Supervision Consultant (CSC/ISAN). An Environmental Compliance Certificate (ECC) for the Project (both Lot 1 and Lot 2) from the Ministry of Natural Resources and Environment (MONRE) was obtained on 27 October 2021, which remains valid throughout the project implementation. (see Attachment 2). Since the proposed AW is located within the Lot 2 area, and as per consultation with MONRE, it has been agreed that the 2021 ECC remains applicable for the AW.

3 SUMMARY OF POTENTIAL E&S IMPACTS AND PROPOSED MITIGATION MEASURES

In line with the Environmental and Social Management Framework (ESMF) requirement for the Project (LDRM-AF), a safeguard screening was conducted for the proposed AW of the ODX subproject and the results are provided in Attachment 2. Given the location, activities, and design of the proposed AW, it is expected that the overall impacts and proposed mitigation measures for the AW will be similar to those being implemented for the ODX subproject, and they are briefly summarized in this Chapter.

Given that the contractor and the Construction Supervision Consultant (CSC/ISAN) for the proposed AW will be the same, the existing C-ESMP (signed between DOW and contractor) will be updated to include the mitigation measures for the proposed AW during construction (Table A2-3 in Attachment 2) and the recent plan proposed to strengthen safety and safeguard measures for the ODX subproject (see Attachment 3). At this stage, PIU (assisted by PMU and PTI) has initiated the river bank profile monitoring program and ensure that the solid and liquid waste management is effective in the Subproject area in Nam Mao and Nam Kor.

3.1 Positive Impacts

Similar to the positive impacts identified in the ESMP of the ODX subproject, it is expected that the proposed AW will have overall positive impacts both from environment and social aspects. As mentioned, Oudomxay Province experienced flash floods with devastating effect to infrastructure and loss of life in the provincial capital, Muang Xay, in 1945, 1985, 2008 and 2013. During the most recent flash flood, 93.6 mm of rainfall occurred in one day inflicting heavy damage to infrastructure and loss of life to seventeen persons. Flood damage in the provincial capital was largely caused by inundation of excess water from the Nam Kor River and its tributaries, including the Nam Mao, Nam Sin and Nam Hin.

Implementation of the proposed structural measures (riverbank protection, expanding drainage capacity of Nam Kor River, installation of dike and flood gates, and address the urban flood risk management activities including improving service capacity of the two public parks) will be beneficial for the majority of Muang Xay residents whose properties and livelihoods will be

largely uninterrupted during flood period and can enjoy beautiful environment year-round as well as promote tourism development in the region. It is expected that construction and installation of urban flood risk management infrastructure investment will bring socioeconomic, health and ecological benefits, such as, protecting the river from siltation and sedimentation as a result of runoff and riverbank erosion, and reducing the loss of lives and/or livelihoods caused by flooding. The ODX subproject will help creating an environment of safety, health and well-being for the majority of people who have suffered from the effects of flash flood events in the past. Implementation of the non-structural measures will further strengthen the positive impacts of the ODX subproject, especially during operations of the structural investments.

As part of the LDRM-AF, direct project beneficiaries will include communities in the urban area of Muang Xay, which has a population of 98,000, with 50 percent estimated to be female. National- and local-level project stakeholders will benefit from capacity and institution-building activities. Indirect project beneficiaries include (a) travelers passing through Muang Xay, the most important traffic junction in northern Lao PDR; (b) the country's population benefitting from more reliable, actionable, and better communicated weather, climate, hydrological, and early warning information; and (c) the wider population of Lao PDR benefitting from improved institutional capacities for disaster risk finance and integration of DRM into sector strategies. Implementation of the proposed AW will enhance effectiveness and sustainability of the subproject structures and/or improve safety of Lot2 facilities.

3.2 Negative E&S Impacts and Proposed Mitigation Measures (MMs)

Overall. The overall risk/impact of the ODX project is considered moderate and they can be mitigated during implementation of the subproject.

The applicable national legislations and WB's Safeguard Policies triggered for the proposed AW as well as the environmental and social conditions will be the same as those provided in Chapters III and IV, respectively, of the ESMP of the ODX subproject. Results from the safeguard screening, suggested that the impacts during preconstruction and operations of the proposed AW will be minor and the proposed mitigation will be part of the safeguard activities being prepared, completed, and/or implemented for the ODX subproject.

The proposed AW will not require land acquisition and resettlement of private land and structures and will not affect more ethnic group, thus, preparation of any resettlement plan (RAP) and/or ethnic group engagement plan (EGEP) will not be required as the existing EGDP covers the village where the proposed AW will take place. During construction main impacts will include increasing health and safety of workers and local communities, generation of dust, noise, vibration, and other pollution (air/water), soil erosion and sediments transport, impacts to river hydrology, local traffic, spoil disposal, and construction wastes; and disturbance of local resident and other social impacts related to worker camps and subproject staff. During operations, the impacts will also be part of the overall positive with measures to address riverbank profile and waste management in Nam Mao and Nam Kor. The summary of impacts and mitigation measure is presented in the following sections while the detail is provided in Attachment 2.

3.2.1 Impacts and Proposed MMs during Pre-construction and Operations Phase

RAP/EGEP. It is widely recognized that the previous Environmental Impact Assessment (EIA) for the original Lot 2 subproject identified 266 land plots with 151,515m², primarily agricultural (179 plots with 139,687m²), that were expected to be impacted by the project. As a result, the Resettlement Action Plan (RAP) developed and implemented for the original Lot 2 includes compensation for the land to be affected by this Additional Work (AW). Therefore, the preparation of a new RAP and a new EGEP will not be required, as the existing EGDP already covers the village where the proposed AW will take place. However, the gender mainstreaming and integration will be promoted into the planning, implementation, and M&E. At subproject level, effort is being made in Muang Xay, Oudomxay Province to encourage active participation of women in the planning and implementation of environment and social safeguard measures on the ground. Under ODX subproject, an effort will be made to strengthen the planning, implementation, and monitoring of the pilot activities that can be carried out by local authorities with active engagement of local communities on the use of nature-based/green infrastructure solutions and waste (solid and liquid) management to reduce pollution along the waterways.

UXO technical survey. Before construction of the ODX subproject, a UXO technical survey was conducted before construction under Lot 1 and Lot 2 and no UXO and/or any explosives were found. In this context, for the proposed AW, no action on UXO risk will be conducted, however, the UXO risks will be included as part of the safeguard training to ensure that workers and staff of contractor are well-aware of the UXO risks.

Other impacts during preconstruction. This will be related to safety and other construction issues and the proposed mitigation measures for construction which have been included in the C-ESMP will be applied. Consultation with local authority/local community (LA/LC) was also conducted during the drafting of this report and key activities are provided in Chapter 4 of this report.

Impacts and mitigation measures during operation phase. As mentioned in the ESMP of ODX subproject, it is expected that operations of the proposed ODX subproject will not create adverse negative impacts during operation phase and the main impacts may create unexpected impacts (erosion and/or deposition) in the upstream and/or downstream of the subproject facilities as well as create safety risks to the water users (including swimming) and/or waste discharge into the Nam Kor and Nam Mao subproject.

The proposed mitigation measures will include the followings (see details in Table A2-2 in Attachment2):

- Establish and monitor the riverbank profile upstream and downstream of the subproject facilities. At present, PIU/DPWT assisted by PMU/PTI has established a plan and baseline for monitoring the river bank profile before and after the subproject structures (to be identified/confirmed) and this river bank monitoring plan will be expanded to cover the proposed AW area.

- To address safety in the Nam Mao and Nam Kor Rivers, PIU/DPWT will install adequate number of permanent warning signs (visible day and night) upstream and downstream of the subproject structures (at specific sites identified in the Site Safety Map for ODX (see Attachment 3).
- To address issues related to wastes to be discharged into the Nam Mao and Nam Kor Rivers, PIU/DPWT will establish and implement an effective waste management plan to ensure effective management of solid wastes along the subproject area. Table A2-2 will be implemented and closely monitored by PIU/DPWT with guidance from PMU/PTI.

3.2.2 E&S Impacts and Proposed MMs during Construction Phase

(a) *Summary of Potential Negative Impacts*

Key issues and potential impacts on local environment of the proposed AW of ODX subproject due to changing the river profile, widening of the river, and excavation of the riverbed, disposal of sediment and spoil, and transportation of construction materials will include, but not limited to, the followings:

- ***Degradation of local resources and environment as well as increase local traffic and damage local road conditions:*** Project implementation will require large amount of natural resources (stone, sand, laterite, soil, etc.) extraction and transportation and these will create moderate impacts on local resources as well as on road traffic since the Project use of the road infrastructure and may increase local traffic congestion and also degradation of road structures during construction.
- ***Increase noise, vibration, and air quality (mostly dust):*** The Project impacts will occur as a result of construction activities as well as transportation of construction materials and/or demolition of building, facilities, and/or other structures.
- ***Increase sedimentation due to runoff from construction areas:*** The removal of vegetation and earthworks on the river banks and in-stream works will detach sand, silt, and clay which will be suspended in the water column for eventual deposition downstream in areas where water velocity slows with the finer fraction likely flowing the length of the lower Nam Kor and Nam Mao Rivers and discharged to downstream areas during the rainy season.
- Excavation of borrow areas, rehabilitation of borrow pits, encountering archaeological sites during earth works, aesthetic/scenic quality, and other disturbances to local communities.
- ***Increase generation of construction wastes, garbage and refuse:*** These wastes including waste oil and chemicals should be contained on site and ultimately disposed of off-site in an environmentally acceptable manner.
- ***Occupational and Community health and safety of local communities and general public:***

- **Traffic, road safety and river access safety:** Haul truck drivers and other staff driving to and from the Project sites may be exposed to traffic conditions, unsafe drivers, poor quality road conditions, pedestrians and other obstacles, etc. that may lead to accidents and injury that may lead to accidents and injury to local people, road user, pedestrians or bicycle rider in settlements, project sites and between settlements along the hauling route for construction materials / disposal of construction wastes. At present, PIU has issued a local order to prohibit public access into the subproject construction sites. Special attention and effort will be paid to prevent outside people and children from entering, playing around and swimming along the river and river bank.
- **Noise and dust** pose risks for impacts ranging from nuisance level to serious health impacts;
- **Injury / death** to local people, road user, pedestrians or bicycle rider in settlements, project sites and between settlements along the hauling route for construction materials / disposal of construction wastes; As a result of the fatality incident of one 8-years boy drowning in the Nam Kor River (Lot 2 construction area) on 17 June 2023 around 13:00, the contractor together with CSC/ISAN and PIU is ongoing improvement of safety measures to avoid and prevent future incident. The same safety measures will also be applied for the AW and included in Attachment 3.
- Potential for introduction or increased incidences of communicable and infectious diseases resulting from the influx of construction workers into the region.
- **Physical Cultural Properties and Sensitive Area:** The ODX subproject is not expected to create any impacts on any local and national archaeological, paleontological or cultural significance. However, there is a possibility for (as yet undiscovered) sites of local cultural significance (i.e., artifacts, sacred sites, cemeteries) in subproject areas during construction. This will be confirmed during the preparation of the C-ESMP. Nonetheless, a “Chance Finds Procedure” describing the process to be followed when any artifacts are found has been incorporated into the Attachment 2.

More details on the risks and impacts are presented in Attachment 2.

(b) Avoidance, Management and Mitigation Measures

To mitigate these impacts and facilitate effective implementation, the present ESMP has been prepared by identifying key issues, proposing mitigation measures, and monitoring indicator are provided in Attachment 2. Attachment 2 also identifies typical actions/mitigations to reduce impacts such as generation of dust, noise, vibration, safety, waste, water quality, erosion and sediment, and social aspects including “chance finds procedure” and environmental, social, health, and safety (ESHS) aspects. Attachment 3 provides information on the recent plan to strengthen safety and safeguard measures

Consultation with local community and implementation of grievance redress mechanism (GRM) will also be required. During construction, under CSC/ISAN supervision and

monitoring, contractor will be required to regularly conduct consultations with local community and report to PIU as monthly basic. The Project and contractor will also be required to establish and implement a GRM system. Monitoring and reporting of contractor performance on the GRM implementation and tracking forms will be provided in the ESS monitoring reports. Grievance related to safeguard issues from communities, poor and vulnerable groups and other stakeholders that result from project activities will be resolved by the Grievance Redress Committee (GRC). However, the complainant also retains the right to bypass this procedure and can address a grievance directly to the PMU Office or the National Assembly, as provided for by law in Lao PDR. At each level, grievance details, discussions, and outcomes will be recorded in a grievance logbook. The status of submitted grievances and grievance redress will be reported to Project Manager through the monthly reports. The GRM for project execution issues is contractor driven and needs to be manned by personnel with appropriate skills.

(c) Preparation of an updated C-ESMP

After the contract signing for ODX subproject Lot 2, the contractor prepared a Contractor-ESMP (C-ESMP) for the ODX subproject (Lot 2) and it was approved by DOW². To address the safety and safeguard issues observed in late 2023, the CSC/ISAN prepared a plan to strengthen safety aspect and other safeguard measures for the ODX subproject and WB provided comments in late May 2024. To ensure effective and timely implementation of the proposed safety and safeguard measures, DOW will update the C-ESMP to include the measures identified in Table A2-2 Attachment 2 and the proposed plan to strengthen safety and safeguard measures proposed in Attachment 3.

4 CONSULTATION, INFORMATION DISCLOSURE AND GRIEVANCE REDRESS MECHANISAM (GRM)

4.1 Consultation and Information Disclosure

During the preparation of the ESMP of the ODX subproject, a number of consultations with affected communities have been conducted during 11-29 May 2020 with a total number of 747 participants, of which 287 are females. Participants are from affected households (men, women, ethnic minorities, and disadvantaged people), village authorities, and representatives from Muang Xay Women's Union, Lao National Front for Development and Provincial Public Work and Transport. The results were included in the ESMP report of the ODX subproject while the ESMP was also disclosed in MPWT website. Additional consultation with village authorities was carried out in February 2024 and summary of consultation results is presented

² The C-ESMP comprises a number of specific plans including Occupational Health and Safety Plan (OHSP), Community Health Safety Plan (CHSP); Site clearance and Revegetation plan; Labor Influx and Labor Management Plan (LMP) – COC; Works/Worker Camp Management Plan (WCMP); Construction Sites Management Plan (CSMP); Construction Materials Management Plan (CMMP); Waste Management and Recycling Plan (WMRP); Traffic and Transportation Management Plan (TTMP); Environmental Quality Management Plan (EQMP); Project Change Management Plan/ Adaptive Management Plan (AMP); Emergency Preparedness and Response Plan (EPRP); Monitoring and Reporting Plan (MRP).

in Table 4-1 below. Consultation and involvement of LA/LC on safeguard activities will be regularly carried out during the construction of the ODX activities. During the implementation of the proposed AW, additional consultation and information disclosure will be conducted by PMU/PTI in close cooperation with the CSC and contractor. Table 4-2 presents a future consultation and information disclosure plan to be conducted during construction phase for the ODX subproject including the proposed AW.

Table 4-1 Summary of Consultation Results

Positive Impacts	Negative Impacts	Recommendations
<ul style="list-style-type: none"> - The Project will benefit Muang Xay residents by safeguarding properties and livelihoods, enhancing the environment, and promoting tourism. These measures will also provide socioeconomic, health, and ecological benefits by preventing siltation, sedimentation, and flood-related losses. - There will be better road networks equipped with drainage and flap gates which will then reduce potential annual floods, as well as reduce riverbank erosion; - The city landscape will be more attractive with public parks and recreational areas for outdoor activities such as exercises. - Better social connectivity; - Enhance social and community awareness on flood protection. 	<ul style="list-style-type: none"> - The proposed AW project will not involve the acquisition of private land or the resettlement of any structures. - Potential disturbance from construction activities such as noise, dust, domestic wastes, construction waste, sediment, OHS of the workers, CHS from traffic, road safety and river access safety that may lead to accidents and injury to local people, road user, pedestrians or bicycle rider in settlements, project sites and between settlements along the hauling route for construction materials / disposal of construction wastes. - Potential social issues such as robbery. - Some families would lose their houses while some people have only one house/property. 	<ul style="list-style-type: none"> - The project need to ensure the contractor develop and implement all necessary measures to avoid and mitigate the impacts during the construction. - The village and district authorities and the project team will have to work together. <p>All issues and recommendations were addressed and incorporated in this AW-ESMP and will be included in the CESMP.</p>

Table 4-2 Future Consultation Sessions

No.	Consultation	Objectives	Methods	Implementing Body	Timeline
Construction Phase					
1	Consultations with affected communities on subproject activities, impacts, construction schedule and work plan	To inform PAP on subproject construction activity and schedule and environmental	Public meeting	Contractors, CSC/ISAN, DPWTs	Throughout construction phase

No.	Consultation	Objectives	Methods	Implementing Body	Timeline
		and social risks, including risks related to community health and safety			
2	Dissemination of community health and safety with affected communities	To educate PAP on community health and safety and measures to prevent subproject related accidents	Public meeting	Contractor, CSC/ISAN, DPWTs	Throughout construction phase
3	Ad hoc meetings where there are substantial changes that have been made, or conflict has arisen due to accident, misunderstanding, or other causes.	To address or solve conflicts	Open Meeting, Focused Group Discussion and In-depth Interview	Contractor, CSC/ISAN, DPWT, PIU, PTI	Throughout construction phase
Operation Phase					
4	Consultation on Green Clean and Beautiful (GCB) community network on waste management in Xay District (Moving towards GCB for Xay District)	To obtain opinion of local community on participatory GCB community network on waste management	Public Meeting, Focused Group Discussion and Key Informant Interview	PTI/EDPD and DPWT/PIU	O&M Phase
5	Consultation on the riverbank monitoring program (1 km upstream and 1 km downstream of the subproject sites) for at least 3 years after construction is completed.	Same as above	Same as above	Same as above	Same as above
6	Installation of proper warning and safety signs (visible day and night) at the construction sites to ensure safety of other waterways users. Installation of safety utensils to enhance safety for people who come to the embankment for	Same as above	Same as above	Same as above	Same as above

No.	Consultation	Objectives	Methods	Implementing Body	Timeline
	recreational, fishing, or transportation purposes				

4.2 Grievance Redress Mechanism (GRM)

The objective of the Project’s GRM is to provide affected parties/persons with redress procedures that they can conveniently use to raise a project related concern, or grievance. The GRM specifies how a project related complaint can be made, including forms and channels through which a complaint can be lodged.

The Government of Laos PDR has various laws and sub-decrees that have been active to guide the implementation of complaint resolution process. These documents specify the right of the complainants as well as the responsibilities of concerned governmental agencies as to complaint resolution. Relevant legal documents include:

- Law on Complaint (amended version), No. 05/NA dated: 9/11/2016.
- Law on Economic Conflict (amended version), No. 51/NA dated: 22/06/2018.
- The Law on Resettlement and Occupation, No. 086/NA, dated 15/06/2018.

Since the GRM for the BKX subproject has been well established and monitored. With the AW, the principles of the GRM and detailed procedures including those related to the redress procedures for worker conditions, and social aspects will be applied and monitored. The forms for GRM monitoring as well as for accident/incidents reporting will also be the same as those being used for the ESMP of the BKX subproject.

Channel. Different channels are established to enable affected person to submit their grievances, including submission to village committee, as well as district and provincial levels. Grievance can also be submitted to:

- i. **PMU’s email and WhatsApp** administered by PMU as GRM focal point (Table 4-3)
- ii. **GRM posters and boxes** established at contractor office and project affected villages (checked every 2 weeks);
- iii. **Electronic platform** (e.g. WhatsApp group consisting of all affected village chief, PMU and PIU ESS coordinators, CSC and contractor)
- iv. **PIU offices** at DPWT Office;
- v. **PMU, PIU, CSC staff** during meetings or during site visit (Table 4-3);
- vi. **Regular village meetings or consultation** to be regular carried out during construction phase.

Disclosure. Since the AW is located in villages covered by the existing Lot 2 area, GRM procedures were disclosed in the affected villages, and GRM boxes were established in each affected village and community area. Also, the GRM procedure was explained to those attending the consultation meetings

Documentation. A grievance logbook will be maintained at PIU (subproject level) and at PMU level (through PMU GRM focal point). A grievance logbook is established and regularly updated/maintained at contractor office and PIU level.

The project will maintain four complaint handling procedures for four types of potential grievances, including grievances related to 1) land acquisition/economic displacement, 2) labor and working conditions, 3) sexual exploitation and abuse and sexual harassment (SEA/SH), and 4) environmental impacts (during construction). All four grievance redress procedures come under the overall management of the PMU's Grievance Focal Points who oversee the receipt of, and resolution of all grievances submitted under the Project. The four grievance redress procedures are summarized below.

(a) Redress Procedure for Complaints related to Land Acquisition/Economic Displacement

In case any complaints related to land acquisition could be resolved through the following Steps. However, as part the law in Lao PDR, the complainant retains the right to bypass this procedure by addressing their complaint directly to the PMU office or the national assembly.

- **Stage 1:** If PAP and PAH are not satisfied with the resettlement plan or its implementation, PAP and PAH can issue a verbal or written complaint to the village committee or the district resettlement office (DRO). If it is a verbal complaint, the village should deal with this complaint and document the grievance immediately at the time. The village committee or DRO should resolve the complaint or grievance within two weeks;
- **Stage 2:** If the PAP and PAH are not satisfied with the result of Step 1, PAP and PAH can file an appeal with the project resettlement office (PRO) after PAP and PAH receives the decision made in Step 1. The PRO should make a decision within two weeks;
- **Stage 3:** If the PAP and PAH are not satisfied with the result of step 2, PAP and PAH can file an appeal with the provincial resettlement committee for administrative arbitration after receiving the decision made by the PRO. The administrative arbitration organization (AAO) should make the arbitrated decision within 10 days; and
- **Stage 4:** If the PAP and PAH are still unsatisfied with the arbitrated decision made by the AAO, after receiving the arbitrated decision, PAP and PAH can file a lawsuit in a civil court according to the relevant laws and regulations of Lao PDR.

PAP and PAH can make a complaint or appeal on any and all aspects of project design and implementation, including issues related to resettlement. A hotline phone will be established with dedicated staff assigned to so that PAPs or complainants will not be charged for their phone calls used to raise their grievances and their complaints are responded in an efficient manner. PAP and PAH will be clearly informed of the complaint and grievance redress mechanism and appeal channels described herewith through village meetings and other channels. In addition, a complaint box should be made available in a convenient location of the village.

The Project and organizations addressing the PAP and PAH complaint and appeal process shall not charge fees. Any expenses incurred due to submission of complaints and/or appeals and phone calls should be classified as unexpected expenses and paid for by the Project.

(b) Redress Procedure for Complaints related to labor and working conditions

Project workers can lodge their grievance/complaint as follows:

- **Step 1 – Employer Level/Contractor.** Affected person (AP) can submit their grievance to their Employer who serves as the first focal point for receiving and resolving grievance. Grievance can be lodged verbally or in writing, in person or by phone, text message, mail or email (anonymous complaint is accepted). The Employer involved will resolve the case as soon as possible or no later than 15 days. Once resolved and the AP is satisfactory, the Employer will report the case, including resolution process and results, to the PMU for information and record. If the AP is not satisfied with the resolution of their Employer, the Employer will refer the AP to the GRM focal point of PMU, and PMU if needed, and inform the AP of this referral. It is noted that if a complaint is concerned of the safety and health of one or several individuals, such complaint shall be resolved as soon as possible – depending on the nature and urgency of the grievance.
- **Step 2 – PIU/PMU level.** PIU/PMU will resolve the complaint referred by the Employer and acknowledge the receipt of the AP’s complaints within two weeks from the date of complaint receipt. If the GRM of PIU/PMU cannot resolve the complaint, the GRM focal point of PMU will consult with the Project Manager for resolution. The GRM focal point of PIU/PMU will inform the AP of the PIU/PMU’s resolution result in writing within 30 days from the date of complaint receipt. If the AP is not satisfied with the resolution outcome proposed by PIU/PMU, PIU/PMU will refer the case to the PIU/PMU for resolving and inform the AP of this referral in writing.
- **Step 3 – Court of Law.** If the AP is not satisfied with the resolution proposed above, a multi-stakeholder committee will be established (ad-hoc) to resolve the dismissed grievance – as an alternative for affected person going to court. If the grievance could not be resolved satisfactorily by the multi-stakeholder committee, the affected person may resort to the court of law. The cost associated to the lawsuit shall be borne by the AP. The decision of the Court will be final.

(c) Redress Procedure for Complaints related to SEA/SH

Under this Project, GRM for SH/SEA mainly serves in: (i) referring the complainants to local Gender-Based Violence service provider; and (ii) recording resolution of the complaint. The following principles, which will be applied under the Project, recognize victim as principal decision makers in their own care, and treat them with agency, dignity and respect for their needs and wishes.

- Multiple channels are in place for easy access and lodge complaints.
- SH/SEA victims will be referred to local SEA/SH service provider for immediate support if they make a complaint directly to PIU/PMU.
- Confidentiality of victims is protected. GM operator of PIU/PMU will keep SH/SEA allegation report confidential.
- No identifiable information on the victim shall be collected and stored in subproject Grievance Logbook.
- Costs of operating the SH/SEA GRM will be financed by the subproject.

Channels for lodging SH/SEA complaints:

- Channel 1 – AP can submit a complaint, verbally or in writing, to Village Mediation Committee/Village Authorities
- Channel 2 – Alternatively, AP can lodge their complaint, verbally or in writing, to GRM Social Focal Point of PMU.
- Channel 3 – AP can submit a complaint to, or seek counselling support from local Lao Women’s Union, as they wish.

All SH/SEA related grievance (no matter which channel through which their complaint is lodged) will be processed and resolved directly by Lao Women Union (LWU) who will be engaged by PMU to assist in addressing potential grievances on SEA/SH.

The Project, and organizations resolving AP complaint, and appeal process, will not charge any fees to affected people. Any expenses incurred due to submission of complaints and/or appeals and phone calls should be classified as unexpected expenses and covered by the Project.

(d) Redress Procedure for General Complaints

In case individuals, households, or communities are affected by any other aspects, for instance, environmental impacts (e.g. dust, noise, or lack of safety measures that increase risks of traffic accident to road users or to local ethnic group), their complaints could be submitted through various channels that will be established for their convenient use, including people from Ethnic Groups. These include:

- PIU and CSC’s GRM focal point’s telephone;
- Local Village Head or/and EG leaders (in case affected individual/households are EG)

The redress procedure follows the same procedure for Complaints related to labor and working conditions.

The communities and individuals who wish to submit their complaints to existing project-level grievance redress mechanism or the WB’s Grievance Redress Service (GRS) could be done by contacting the following relevant staff and organizations as presented in Table 4-3 below:

Table 4-3 GRM Contact Persons

No.	Name and Surname	Position	Organization	Contact Number
I.	PMU Level			

No.	Name and Surname	Position	Organization	Contact Number
1	Mr. Moukmany Vanhnasy	National Project Coordinator	Department of Waterways (DOW) the MPWT	02029122279
3	Mr. Lattiyakorn Phomdaungsy	Project Officer	Department of Waterways (DOW) the MPWT	05055895899
II. PIU Level				
1	Mr. Somchith Panyasack	Provincial Deputy Governor ,Economic Work Leader	Steering Committee	020 918 73334
3	Mr. Phommasouk Monechandy	Muang Xay Deputy Governor	Head of the Secretariat	020 999 80669
4	Mr. Khamdam Phongmany	President of Muang Xay Lao Front for National Development Office	Deputy Head of the Secretariat	020 566 43633
III Village Level				
1	Mr. Khamsang Phatthala	Chief of village	Namy Village	020 58971770
2	Mr. Bounhom Yordbounhak	Chief of village	Nalao Village	020 55581118
3	Mr. Phonekeo Sisongkham	Chief of village	Nawarnoy Village	030 92092041
4	Mr. Somsanith Inthavong	Chief of village	Thin Village	020 55139636
5	Mr. Vanxay Bountapha	Chief of village	Longkordeua Village	020 56526501
IV CSC and Contractor				
1	Mr. Bolisouthchai Phounsavath	Environmental Specialist	CSC/ISAN-LTEC	020 96366659
2	Mr. Ako	Site Manager	Contractor	020 59221118

Table 4-4 below summarizes the GRM activities to be conducted with the BKX including the proposed AW.

Table 4-4 GRM Activities

No.	Activities	Responsive Agency	Timeline
1	Update and redistribute Project Information poster and GRM Log Book to all affected villages	PIU with assistance from CSC and Contractor	Before starting of AW construction activities
2	Inspect and maintain or update (if needed) the installed GRM poster and GRM Boxes (Lao language) of the four GRM procedures presented above.	PMU	Before starting of AW construction activities

No.	Activities	Responsive Agency	Timeline
3	Conduct refresh training on the GRM procedure above to PIU, SMWGs, CSC, Contractors and affected communities	PMU	Before starting of AW construction activities and continue refresh regular trainings thought out the construction phase
4	Conduct training on the four GRM procedure for contractor’s workers and staffs	Contractor	Before starting of AW construction activities and continue refresh regular trainings thought out the construction phase
5	Conduct weekly inspection of GRM boxes	PIU and CSC	Thought out the AW construction phase
6	Document and report on GRM implementation in the ESS monthly and quarterly reports	Contractor, CSC, PIU and PMU	Thought out the AW construction phase

5 MONITORING, REPORTING, TRAINING, AND BUDGET

5.1 Monitoring and Reporting

The implementation arrangement of the proposed AW will be similar to that identified in the ESMP of the ODX subproject. The CSC/ISAN will submit the monthly implementation progress and the quarterly safeguard monitoring reports to DOW (copy PTI) while DOW/PTI will submit the Project Environment and Social Monitoring Report (ESMR) to WB every 6 months. As requested by DOW or the WB, the WB would also review the CSC reports and provide comments as needed.

5.2 Training and Budget

Safeguard Training. After construction is completed, given the risk of river bank erosion upstream and downstream of the subproject sites in Nam Mao and Nam Kor, PIU will be responsible for implementation of the safeguard measures to ensure effective and sustainable operations of the subproject activities including the AW. In this context, more capacity building and/or training will be required to ensure that PIU/SMWG has adequate capacity to implement and monitor the need on the river bank profile for erosion monitoring in the subproject area (Nam Mao and Nam Kor) including ensuring safe operation of the small movable weir in dry season, and waste management to avoid discharge of liquid and solid wastes into the rivers.

Additional safeguard training for the proposed AW will be prepared and implemented by DOW/PTI. This is to ensure that adequate training and budget are allocated for the safeguard implementation including those related to the AW.

Table 5-1 provide an indicative safeguard training plan to ensure that safety and safeguard measures are implemented timely and effectively.

Table 5-1 Training Plan

No.	Training Topic	Trainer	Trainee	Timeline
Objective: To ensure compliance during preparation and construction				
1	Preparation and implementation of the updated C-ESMP –AW focusing on occupational and community health and safety	Contractor	Workers	Before commencement of construction and during construction
2	Community health and safety	EDPD/PTI, PIU	Affected communities	Before commencement of construction, during construction and post-construction completion
To build capacity of DPWT to mitigate impacts during operations and moving towards GCB				
3	GCB community network on waste management in ODX (Moving towards GCB for ODX)	PMU and PIU	DPWT and staff local authority and mass organizations and local community	Training will be made through the learning by doing approach (adaptation). Budget will be provided to initiated the process

On budget, it is estimated that an additional cost of US\$28,000 will be required to cover the cost for training, monitoring, consultants, and consultation with LA/LC and ensure their active participation (see Table 5-2) during the implementation of the ESMP for ODX sub-project including the proposed AW. Details will be included in the annual work plan and budget plan to be allocated by DOW.

Table 5-2 Indicative budget allocation for ESMP training for ODX subproject including the proposed AW.

Key Activities	Two years (2024-2025)		
	Y-2024	Y-2025	Total (USD)
1. Training and workshop for safeguard capacity building for PIU including consultation and information disclosure	2,000	2,000	4,000
2. NESC cost for monitoring, reporting, capacity building to ensure effective implementation of safeguard for ODX (1/3 of the total cost)	8,000	8,000	16,000

Key Activities	Two years (2024-2025)		
	Y-2024	Y-2025	Total (USD)
3. Monitor a gender analysis /CESMP/ to identify gender issues and guidance on project cycle gender mainstreaming, guide development of gender action plan (GAP), during project planning, construction and implementation, and post-project O&M (Conducted by Contractor)	1,000	1,000	2,000
4. Establish and monitor the river bank profile including installation of adequate permanent warning signs in the Mekong River to ensure safety and effective management of garbage during operations of the subproject facilities	2,000	2,000	4,000
5. On the job training and capacity building for safeguard implementation, monitoring, and reporting during operations with PIU, DPWT	1,000	1,000	2,000
Grand Total	14,000	14,000	28,000